

MINUTES OF THE MEETING
LOCAL GOVERNMENT COMMITTEE
MONTANA STATE SENATE

January 17, 1985

The fourth meeting of the Local Government Committee was called to order at 1:00 p.m. on January 17, 1985 by Chairman Dave Fuller in Room 405 of the Capitol Building.

ROLL CALL: All members were present. Senator Regan arrived late. Senator Story was excused at 1:05 and rejoined the meeting before it was over.

CONSIDERATION OF SENATE BILL 88: Senator Gary Aklestad, District #6, is the sponsor of this bill. It was introduced to permit a city under certain circumstances to dissolve its police retirement fund and to transfer money contained in the fund to the city general fund.

PROPOSERS

James Johnson, the Shelby City Attorney, spoke in favor of the bill. He stated that since the City of Shelby consolidated their law enforcement program with the County, there is a balance of approximately one hundred and forty one thousand dollars in the former police retirement fund. He stated there will be no further claims against the fund. He said the money cannot be utilized for any other purposes and cannot go to the city general fund without authorization.

OPPOSERS

There were no opposers to SB 88.

Questions from the Committee were called for.

Senator Regan asked if there was some way to determine how much of the fund was state money. Mr. Johnson stated that it would be difficult. He thinks most of the money is from local taxes.

Senator Crippen asked how long a police officer had to be on the police force before being eligible for disability benefits. Mr. Johnson said it was five years.

Senator Mohar asked if anybody not yet eligible to collect benefits who had contributed to the fund had received their money back. Mr. Johnson said they had.

Chairman Fuller and other senators expressed their concerns about finding out how much of the money in the fund is state money and whether that portion of the money should not go back to the state general fund.

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Chairman Fuller stated he felt this may be an incentive to local governments to close out their retirement funds. Senator Aklestad stated that this was not the intent of the bill.

Senator Crippen expressed his concerns about future claims against the fund. Mr. Johnson stated that there was only one police officer who stayed with the consolidated force and he signed a release on the fund. He felt there would be no further claims against the fund.

Senator Crippen requested that the staff researcher, Karen Renne, research the bill and make sure there are no state dollars in the fund.

The hearing was closed on SB 88.

CONSIDERATION OF SENATE BILL 92: Senator Chris Christiaens, District #17, is the sponsor of this bill. It was introduced to extend handicapped persons' parking privileges to persons who are entitled to such privileges in another state.

PROPOSERS

Robert Liston, representing the Governor's Committee on Employment of the Handicapped, spoke in favor of the bill. He stated the International Symbol of Access is the only recognized symbol for handicapped parking and it would be consistent to use this symbol to recognize out-of-state handicapped parkers. His written testimony is attached as Exhibit A to these minutes.

OPPOSERS

There were no opposers to SB 92.

Questions from the Committee were called for.

Senator Regan asked what the International Symbol of Access is. Mr. Liston stated it is a wheelchair.

The hearing was closed on SB 92.

CONSIDERATION OF SENATE BILL 93: Senator Chris Christiaens, District #17, is the sponsor of this bill. It was introduced to allow local governments to establish requirements for handicapped persons' special reserved parking spaces. Senator Christiaens stated he would like to propose an amendment to the bill requiring a sign in handicapped parking places also. Senator Christiaens submitted written exhibits on access information and handicapped parking signs. They are attached as Exhibit B to these minutes.

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PROPONENTS

Robert Liston, representing the Governor's Committee on Employment of the Handicapped, spoke in favor of the bill. His written testimony is attached as Exhibit C to these minutes.

Zana Smith, representing the Montana Independent Living Program, spoke in favor of the bill. She stated that many handicapped parking spaces are not functional because they are not located near curb cuts and the spaces are not wide enough to accommodate specially equipped vans.

OPPONENTS

There were no opponents to SB 93.

Questions from the Committee were called for.

Senator Crippen asked about the space width requirement of eight feet in the bill and asked why they don't request to make it twelve feet. Senator Christiaens said that would be great and would check into whether the space width requirement was from federal guidelines.

Senator Harding asked for clarification regarding the clause in Section (4) of the bill which states "the governing body of a city, town, or county may impose all, but not less than all, ...". Karen Renne will ask the bill drafter what was meant by the clause.

Senator Christiaens closed by stating there is a continuing need for education for things that will help the handicapped lead more full and productive lives. He urges the Committee's support of this bill.

Chairman Fuller stated the bill would be given to the staff researcher for a drafted amendment regarding the sign and further clarification of parking space requirements.

The hearing was closed on SB 93.

CONSIDERATION OF SENATE BILL 94: Senator George McCallum, District #26, is the sponsor of this bill. The bill was introduced to provide that the term of a person appointed to fill a vacancy in the office of Justice of the Peace or City Commissioner terminates as of the next general election and qualification of successor; and to conform the term of appointment of these offices with other elective offices.

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PROPONENTS

Jim Jensen, representing the Montana Magistrates Association, stated they were in favor of the bill.

Alec Hansen, representing the Montana League of Cities and Towns, spoke in favor of the bill and stated the statutes regarding these offices need to be consistent.

OPPONENTS

There were no opponents to SB 94.

Questions from the Committee were called for.

Senator Crippen asked if this bill involved county commissioners. Mr. Hansen stated that it only affected city offices.

Chairman Fuller expressed his concern that there were people who were appointed with the idea that they could serve out their full term and perhaps a grandfather clause should be included in the bill for those people. Senator McCallum stated he would have no objections to doing that.

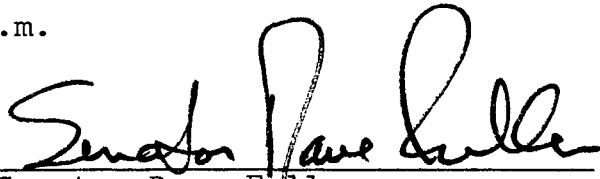
Senator Mohar asked for clarification of the term "remainder of unexpired term."

Karen Renne will look into clarification of unexpired term and the matter of a grandfather clause.

The hearing was closed on SB 94.

ACTION TAKEN ON SENATE BILL 92: Senator McCallum moved that the Committee recommend a DO PASS on SB 92. The motion passed unanimously.

The meeting adjourned at 2:10 p.m.



Senator Dave Fuller
Chairman

ROLL CALL

LOCAL GOVERNMENT

COMMITTEE

49th
~~XXXX~~ LEGISLATIVE SESSION -- 1985

Date 1-17-85

SENATE
SEAT
#

NAME	PRESENT	ABSENT	EXCUSED
13 Senator Crippen, Bruce	✓		
18 Senator Eck, Dorothy	✓		
11 Senator Harding, Ethel	✓		
47 Senator Hirsch, Les	✓		
4 Senator McCallum, George	✓		
28 Senator Mohar, John (V.Chair)	✓		
44 Senator Pinsoneault, Dick	✓		
19 Senator Regan, Pat	<i>Pete</i> ✓		
21 Senator Story, Pete	✓		
43 Senator Fuller, Dave (Chair)	✓		

Each day attach to minutes.

STANDING COMMITTEE REPORT

January 17 19 85

MR. PRESIDENT

LOCAL GOVERNMENT

We, your committee on.....

SENATE BILL

92

having had under consideration..... No.....

FIRST

WHITE

reading copy ()
color

HANDICAPPED PARKING PRIVILEGE FOR OUT-OF-STATE PERMITTEES

SENATE BILL

92

Respectfully report as follows: That..... No.....

DO PASS

~~XXXXXXXXXX~~
DO NOT PASS

Senator Dave Fuller

Chairman.

LOCAL GOVERNMENT COMMITTEE
January 17, 1985
EXHIBIT A

SB 92

I am here today in support of Senate Bill 92, which would allow special parking privileges to disabled persons who display a handicapped parking permit from a jurisdiction other than Montana.

Parking reciprocity is an important issue across the country, with many states introducing similar bills. This is primarily in response to the revised Uniform Vehicle Codes which are intended to provide uniformity and reciprocity of parking privileges to disabled persons among various states. These were changed because many states would not accept the parking permit of neighboring states. I have heard people say there is no need for this type of bill because policeman in Montana do not ticket out of state travellers. This is probably true in most cases but I have heard of people who have been ticketed in Montana and had valid handicapped parking permits from other states. With the large tourism trade in Montana, I feel we need to do everything possible to welcome our visitors.

I think one way to make this bill easier to administer and provide more conformity to the Uniform Vehicle Codes would be to state the International Symbol of Access is the only recognized symbol for handicapped parking. Police officers would not need to know the various special plates places like California have. I believe most states are heading this way already.

Again, I am in support of Senate Bill 92, although I feel these minor changes would make it easier to administer. If you don't have a copy of the changes to the Uniform Vehicle Codes, I would be happy to provide this information to you. Thank you for this time to testify.

Robert D. Liston

Robert D. Liston

513½ State St.

Helena, MT 59601

443-1089 or 444-3886

*This is the opinion of the Governor's Committee
on Employment of the Handicapped.*

DB 915

ACCESS INFORMATION BULLETIN



National Center
for a
Barrier Free
Environment

Suite 1006
1140 Connecticut Avenue, N.W.
Washington, D.C. 20036
Phone (Voice or TTY)
202/466-6896 (Staff/Administrative)
800/424-2809 (Information Requests)

CURB RAMPS, PARKING, PASSENGER LOADING ZONES AND BUS STOPS

by
Hollyn Fuller

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CURB RAMPS

Curb ramps benefit not only people with disabilities, but many others as well. Well-designed and well-placed curb ramps are a boon to guardians of children in strollers, the elderly, and persons with wheeled carrying devices. Federal and state laws now require curb ramps to be installed wherever sidewalks or intersections are constructed or replaced.

Alternatives to ramps should be considered. They include:

- Sloping the entire corner to provide a gradual transition between walk and street.

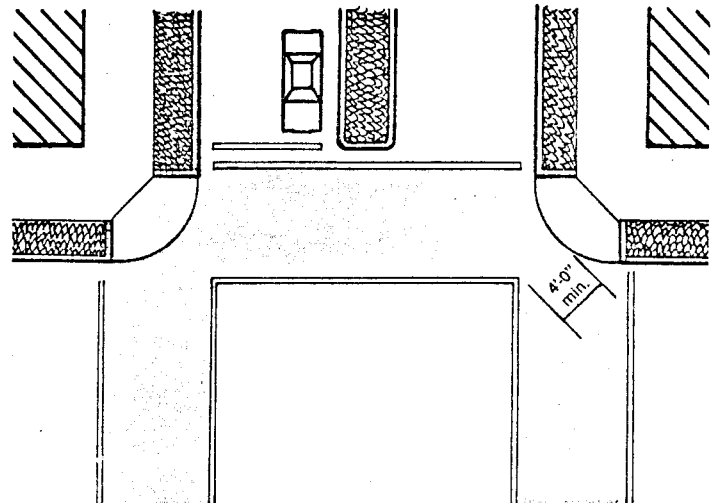


FIGURE 1
SLOPED CORNER CURB RAMP

INTRODUCTION

An accessible interface between pedestrian and vehicular travel is the key to the success of a barrier free environment. Just as access features must be designed into buildings, the disabled traveler must be considered in the design of bus stops, parking spaces and pedestrian pathways. Many states have developed pedestrian paths, parking areas, and transportation systems which provide for disabled drivers, passengers and pedestrians; and additional states are in the process of designing such systems. This bulletin discusses the design considerations necessary for such systems.

- Raising the level of the street up to the level of the sidewalk, requiring cars to change levels rather than people. (In both of the above instances, ground surface materials and/or bollards or barriers must be carefully chosen and designed to separate pedestrian from vehicular spaces.)

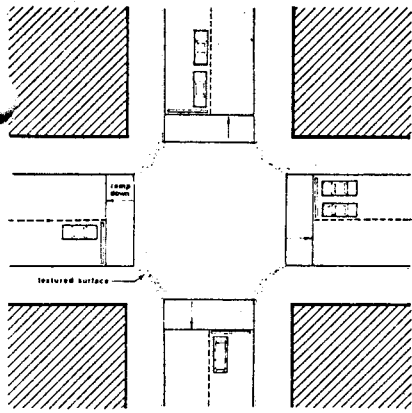


FIGURE 2

RAISED INTERSECTION

- Providing rolled curbs which offer, if correctly designed, a continuous accessible transition between sidewalks and streets.

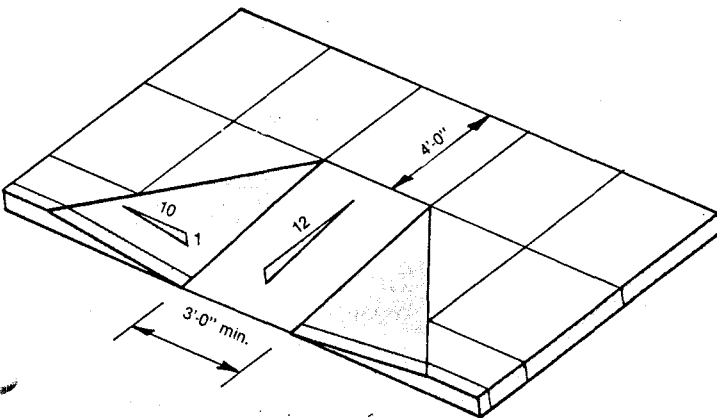


FIGURE 3
STANDARD CURB RAMP

Design: Curb ramps may be integrated into the curb and sidewalk, or built up to the curb from the street. If a ramp is cut into the curb and is located where pedestrians must walk, it should not pose a walking hazard. If a four foot wide walking route is not provided at the top of the ramp, flared sides should be provided on the ramp at a slope of 1:12. It is best to place built-up curb ramps so that they do not project into vehicle paths.

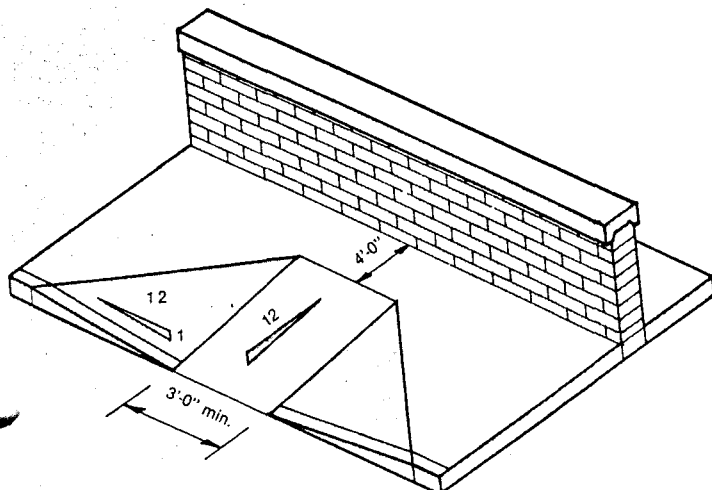


FIGURE 4
CURB RAMP AT NARROW SIDEWALK

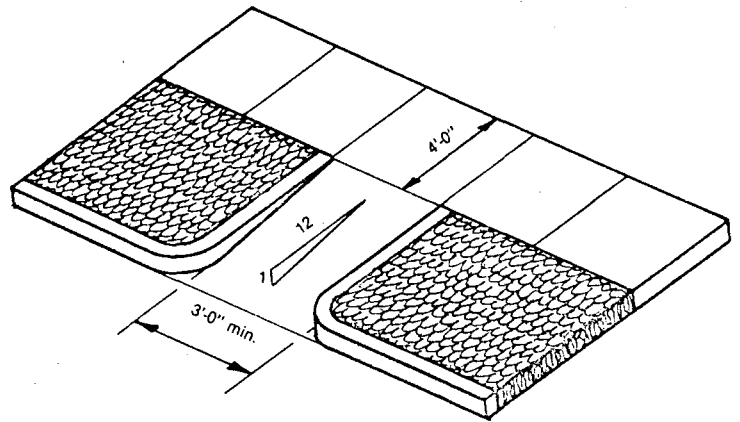


FIGURE 5
CURB RAMP IN BOULEVARD

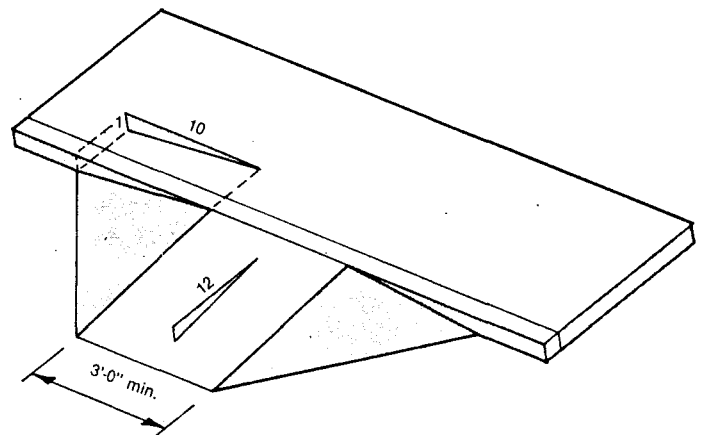


FIGURE 6
BUILT-UP CURB RAMP

Placement: Curb ramps should be located so that pedestrian traffic using the ramp will be able to stay within the marked crosswalks. If a pedestrian pathway crosses a raised island, curb ramps should be provided on both sides of the island. A level area at least four feet (4') long could be provided between the ramps. A cut should be made at street level through the island if it is not sufficiently wide to allow two curb ramps and a minimum four foot (4') landing between.

In addition, the ANSI A117.1 (1980) *Standard for Making Buildings and Facilities Accessible to and Usable by Physically Handicapped People* requires a two foot (2') wide straight segment of curbing within the crosswalk to provide an orientation cue for visually impaired people. See Illustration 7.

If possible, curb ramps should be placed to the side of the primary circulation path, yet still within the crosswalk. Persons with visual limitations may not be able to detect the differentiation between sidewalk and street if a curb ramp is placed directly in the primary pathway. It is, however, important to place ramps where they will not be blocked by parked vehicles.

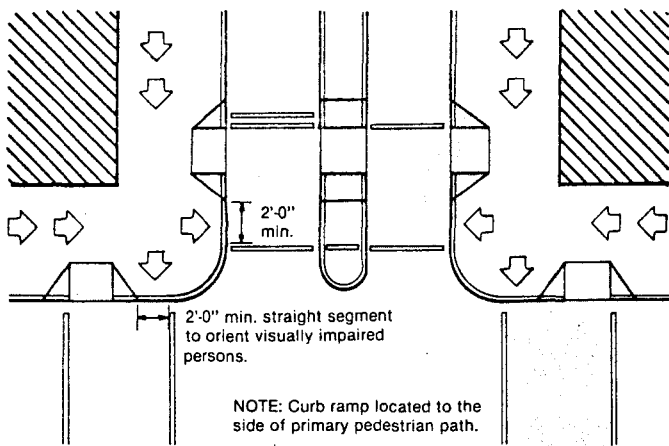


FIGURE 7
CURB RAMP AT NARROW ISLAND

One of the most desirable placements is on the diagonal at the corner. The diagonal ramp is usable for both directions of travel, and the hazardous, up and down, roller coaster effect of two adjacent ramps is avoided. In addition, in parts of the country where sidewalks must be cleared of snow, the diagonal ramps are more likely to be cleared, and thus usable, whereas ramps off to one side may be buried under snow.

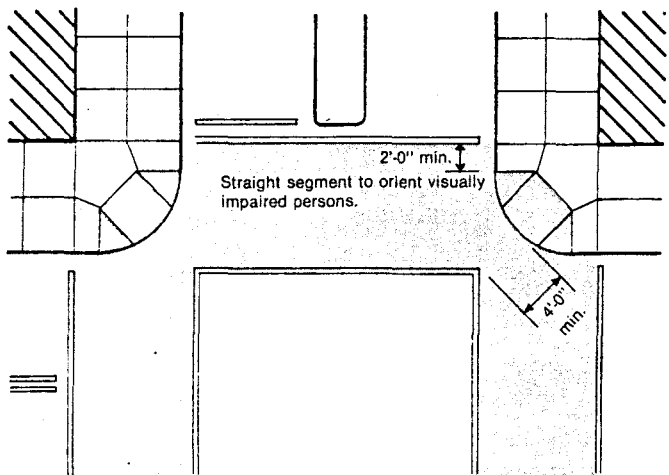


FIGURE 8
DIAGONAL RAMP

Slope: Curb ramps should not have slopes exceeding 1:12. The provision of a 1:12 slope on streets with a steep incline may not be feasible. In such a case, it is best for designers to consult with local code authorities and organizations having people with disabilities to determine an adequate solution. A greater slope may be allowed with the understanding that the curb ramp cannot be used independently.

A cross slope on curb ramps should be avoided. A cross slope may cause a wheelchair to veer to one side. This makes ascent difficult and descent hazardous. A 1:50 cross slope is the maximum slope allowable.

Particular attention should be paid to how the curb ramp fits into the gutter profile and the adjoining street. The foot plates of a wheelchair may catch in the surface of the gutter or street if the angle between the curb ramp and the gutter or street surface is too acute. The United States Architectural and Transportation Barriers Compliance Board, in its *Minimum Guidelines and Requirements for Accessible Design*, requires that, to avoid this situation, adjoining gutters and road surfaces should not have a slope exceeding 1:20.

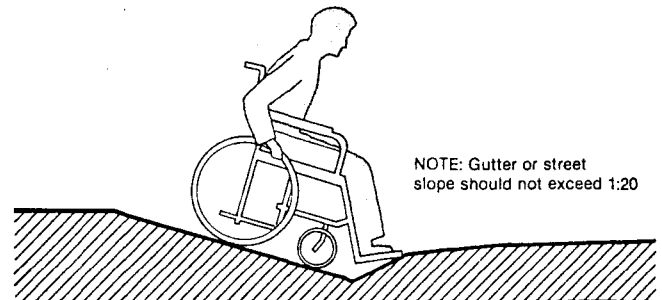


FIGURE 9
HAZARDOUS GUTTER/CURB RAMP PROFILE

Width and Surface: The width of the curb ramp, exclusive of flared sides, should be thirty-six inches (36") to allow for the passage of wheelchairs and other wheeled devices. Surfaces should be smooth and contain no vertical level changes greater than one-half inch ($\frac{1}{2}$ "). A vertical change greater than $\frac{1}{2}$ " may block the ascent of wheelchairs and cause persons using walkers, crutches, or canes to stumble. It is best to have the top and bottom of the ramp blend to a common surface with the street and sidewalk.

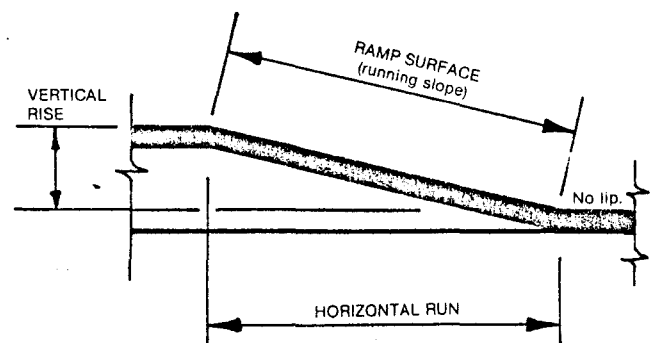


FIGURE 10
CURB RAMP SECTION

Warning Texture: A person with a visual limitation may detect a curb ramp with a cane, by feeling it with his or her foot, or by visually detecting a marked contrast in color or shade. Raised strips, exposed aggregate concrete, or rubber or plastic cushioned additions to the surface will usually be detectable to a person using a cane. If a contrasting color is used, it should also be detectable to a person with limited vision. ANSI A117.7 (1980) specifies that the warning texture extend the full width and depth of the curb ramp, including any flares. Some state regulations specify that a warning texture be provided in a twelve inch (12") band around the perimeter of the curb ramp. If grooves are used as the identifying texture, they should be one-quarter inch ($\frac{1}{4}$ ") wide and deep, and one and one-half inches ($1\frac{1}{2}$ ") on center. A diagonal surface texture will allow for water drainage and thus help to alleviate icing in the winter. Some people with visual limitations prefer a lip where the curb ramp meets the street. As mandated by ANSI A117.1 (1980), the lip should be no more than one-half inch ($\frac{1}{2}$ ") in height with a 1:2 beveled edge. A lip, however, is not recommended for curb ramps that end in a deep gutter, as the lip aggravates the problems described above. For further information on the needs of people with severe visual limitations, refer to the National Center for a Barrier-Free Environment's Access Information Bulletin on *Visual Impairments*.

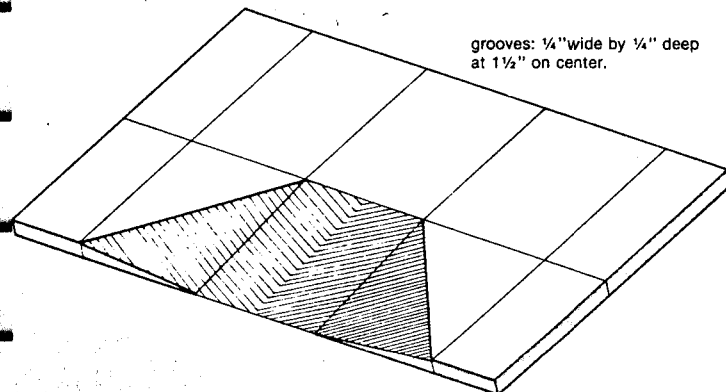


FIGURE 11

CURB RAMP WITH DIAGONAL SURFACE TEXTURE

PARKING

Modes of Travel: Persons with disabilities may drive vehicles with hand controls that are either permanently attached to the vehicle or are portable. Parking spaces designated for disabled people should be designed to accommodate both automobiles and vans. Vans may have rear lifts or a lift on the side opposite the driver.

Parallel Parking: Onstreet parallel parking may be the only available parking in some situations. However, parallel street parking does not offer as many options to the needs of disabled people as

other types of parking. Entering and exiting can be hampered by the curb or other vehicles unless stall locations are carefully planned. Vans with rear lifts need space at the rear of the vehicle which will not be encroached upon by another vehicle. A designated parallel stall should be provided at the entrance end of a street to allow for this condition. In addition, persons in sedans may need to exit on the passenger side, but may be prevented from doing so by the curb. The difference in height between the sedan seat and the seat of the wheelchair when up on the curb may be as much as seven to twelve inches (7"-12"). Such a difference makes the transfer from vehicle to chair difficult, if not impossible, for many people. On a one-way street, a designated stall located on the left side of the street will meet the needs of a disabled person exiting from the passenger side of the vehicle.

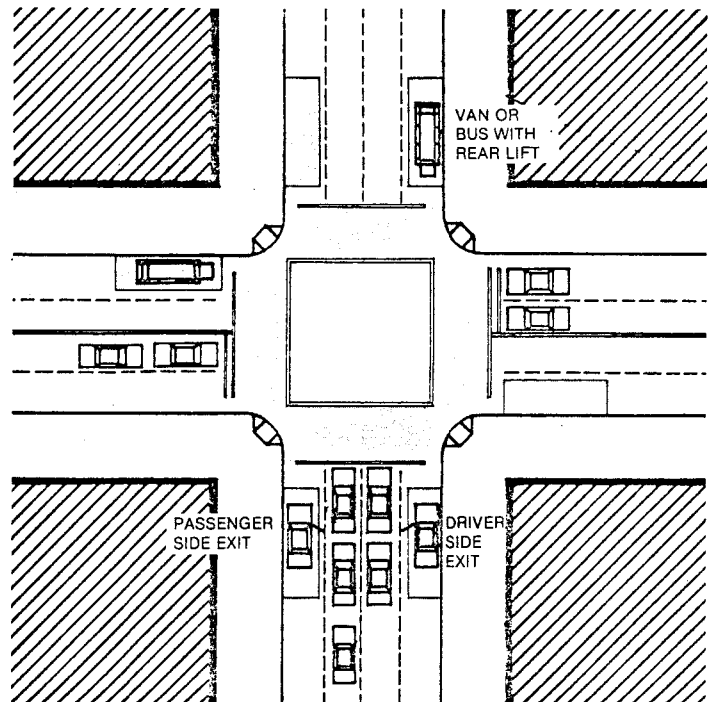


FIGURE 12

ON-STREET PARALLEL PARKING

Vans with side lifts need enough clearance on the curb side to ensure that the lift will extend fully. Stalls should not be designated next to utility poles, trees, ornamental plantings, or other street furniture that would restrict the use of a side lift. In addition, the surface of the area near the curb must be smooth and hard.

Perpendicular Parking: In general, parking should be located as near to building entrances or pedestrian circulation as possible. Physically disabled people often have limited energy resources and thus experience difficulty in traversing distances easily crossed by most able-bodied

people. For some persons, such as those with heart or lung disabilities, placement may not only be a matter of convenience but of necessity. ANSI A117.1 (1980) requires that designated stalls be located on the shortest possible accessible circulation route connecting the parking space and the facility entry. In most cases this distance should be less than two hundred feet (200').

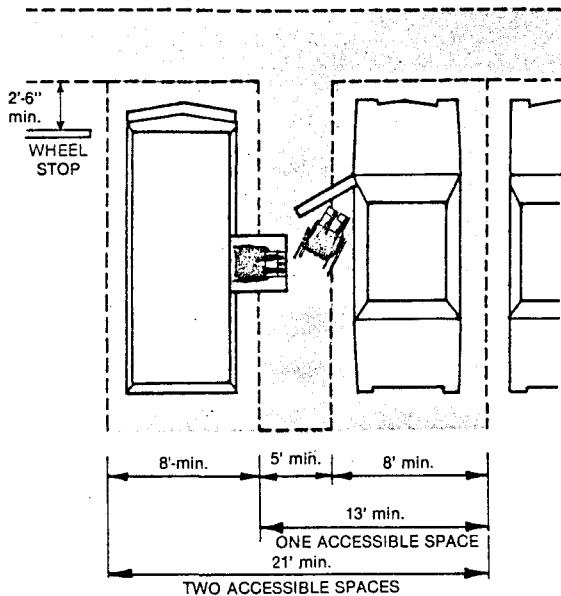


FIGURE 13
ACCESSIBLE PARKING SPACES

Dimensions: Persons using vans with a rear or side lift need space to allow the full extension of that lift. In general, these maneuvers require a space approximately thirteen feet (13') wide and the length of a standard stall, approximately twenty feet (20'). The ANSI Standard, as well as many state codes, allows parking stalls for handicapped persons to be a minimum of eight feet (8') wide, as long as an access aisle is provided adjacent to the accessible stall which is a minimum of five feet (5') in width. Two parallel stalls may share a common access aisle, as shown in Illustration 13. It is important to note that if one of the stalls is occupied, in order to deploy the side lift, a van driver may find it necessary to back into a designated space. Similarly, an automobile driver may find it necessary to back into the space to unload on the correct side. Thus, stalls which share an access aisle must be located to allow either forward or backward vehicle parking. Where accessible parking is provided in garages, a nine foot, six inch (9' 6") clear height should be provided for van headroom along the entire route between entry, parking spaces, and exit.

It is important to provide a barrier between the parking stall and the adjacent accessible circulation route to prevent vehicle overhang from blocking the aisle. A thirty-six inch (36") wide route is the required minimum. See Illustration 14.

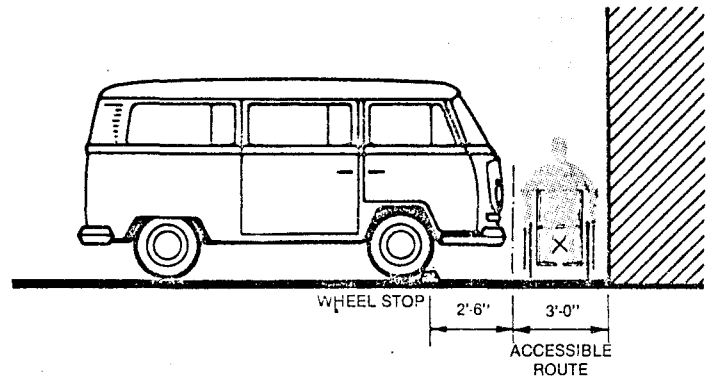


FIGURE 14
ACCESSIBLE ROUTE THROUGH PARKING AREA

Grade and Surfaces: Accessible parking stalls should be designated on as level a surface as possible, because persons using mobility aids have difficulty on sloped surfaces. Wheelchairs may roll out of reach, or the grade may make uphill transfer difficult or impossible between vehicle and chair. Persons using crutches or a walker may slip or lose their balance on a sloped surface. Vanlifts are often not usable on a sloped surface because, when fully extended, the edge of the lift may have poor contact with the ground, creating a drop of several inches. In general, the surface of the parking space should not slope in excess of 1:50, approximately one-quarter inch ($\frac{1}{4}$ ") per foot in any direction.

Surfaces of parking stalls and adjacent walkways should be stable, firm, and slip resistant. Irregular paving and flooring material may cause tripping or difficult wheelchair passage. A gravelled surface is generally not usable by most people with disabilities. For further information on walking surfaces, see the National Center for a Barrier Free Environment Access Information Bulletin: *Ramps, Stairs, and Floor Treatments*.

Signs: Stalls for people with disabilities should be designated by a sign showing the international symbol of accessibility. The sign should be mounted on a wall or a post at the stall location. The international symbol of accessibility indicates that a facility is designed for use by people with disabilities. It is a white stick figure on a blue background. The symbol should be four inches (4") in height when viewed from distances up to sixty feet (60'). Signs should be located approximately five to six feet (5'-6') above the ground so that they are clearly visible to the driver searching for the accessible stall above the adjacent parked cars. To prevent hazards for persons with sight limitations, signs located on or near pathways should be at least eighty inches (80") above the ground. The entrance to offstreet

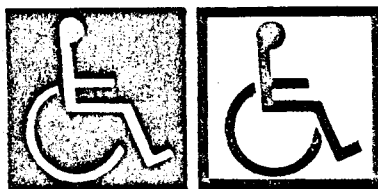
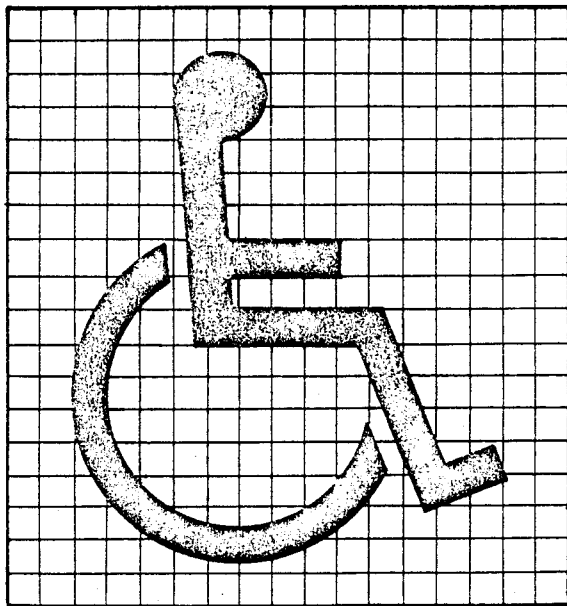


FIGURE 15
INTERNATIONAL SYMBOL
OF ACCESSIBILITY

parking lots should have a sign exhibiting the accessibility symbol at the entrance. It is important that people with disabilities know if they will be able to use the parking lot, especially if there is a fee.

Some states paint curbs different colors to indicate usage. Painting the curb blue, as in the international symbol of accessibility, or yellow, as in no-parking zones, will help to alert most drivers to the special designation of the space. In some locations where violation of the use of the space is a problem, cross hatching of the entire stall with yellow lines has proven to be effective. The painting of the international symbol of accessibility (three feet square) on the ground may be used in addition to the upright sign. But, because it is easily obscured by sand, dust, or snow, it should not be the only means of designation.

Quantity: ANSI A117.1 requires a "reasonable number, but always at least one" accessible parking space. The United States Architectural and Transportation Barriers Compliance Board's *Minimum Guidelines and Requirements for Accessible Design*, combined the requirements from any state and local building codes and developed the following table:

Total Parking in Lot	Required Minimum Number of Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1000	2% of total
Over 1000	20 plus 1 for each 100 over 1000

The quantity, of course, should be sufficient to meet the needs of users. Some facilities have people with disabilities as more frequent visitors. In these cases, more spaces than those specified above will be needed. The above table represents an adequate minimum for most types of facilities.

License Plates and Placards: In many states, persons with disabilities who have drivers' licenses, as well as disabled passengers, can qualify for car license plates or placards. These identify their eligibility to utilize parking spaces designated for handicapped people. In addition, some states allow persons with distinguishing license plates or placards to park their vehicles at parking meters without charge.

License plates indicating the right to park in stalls designated for people with disabilities may be recognized by display of the access symbol or by special letters such as DP or HP (for disabled persons), or VET or VT (for disabled veterans). A special sticker issued by departments of motor vehicles also may indicate the eligibility status.

Sometimes placards made of a durable material exhibiting the international symbol and the words "Disabled Person" are used. The placard is placed on the dashboard, or otherwise located so it is easily visible when in use. The placard may be used by disabled drivers and passengers as well as parents or guardians of children with disabilities.

People with disabilities usually prefer to have both a distinguishing license plate and a placard. The advantage of placards is that they may be transferred from car to car, thus enabling a disabled passenger, or a driver with portable hand controls, to utilize designated parking spaces in more than one vehicle. Some disabled people prefer the placard because they feel that there is a stigma attached to constant display of the symbol, as well as the potential for victimization by those who think that a disabled person may be less able to defend himself/herself. On the other hand, some people prefer the license plate over the placard, since it is permanently attached to the vehicle and can't get lost.

Enforcement: The design of any successful parking system for disabled people, whether on public or private property, will include not only consideration of the disabled users, but also consideration of the general population which must support the system. Designers of accessible parking systems should initiate an educational program. Hopefully, this will encourage people to honor the specially designated parking spaces by not parking there unless they have the distinguishing license plates or placards. This will make enforcement easier for all concerned.

In some states, the enforcement policies of the special designation system must be displayed along with the international symbol on the sign at the parking stall. The sign may state that unauthorized vehicles not displaying distinguishing placards or license plates will either be towed away at the owner's expense or cited. Citations usually can be issued for violations on publicly-owned parking lots.

Local laws may provide for enforcement of specially designated parking stalls in privately-owned parking lots. However, in most of these private lots, it will be up to the owner of the lot to provide enforcement by notifying the local law officials, or by having the offending vehicle towed away at the owner's expense. The political environment may be such that the owner will be reluctant to do so. A public education program may assist the owner in enforcing laws because the public understands and supports the system. For further information on the enforcement policies of your local area, contact your state department of transportation or public safety.

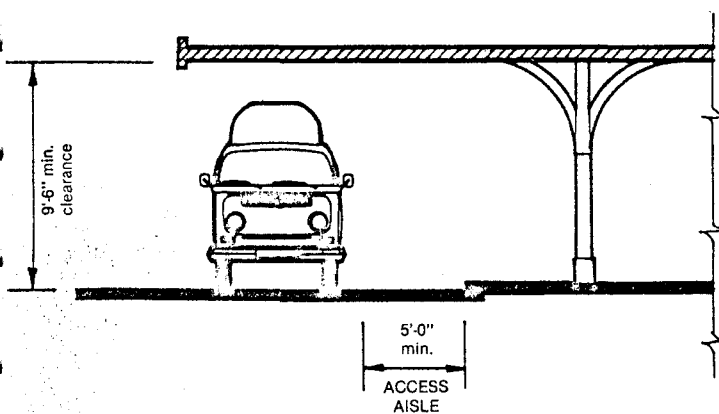


FIGURE 16
VERTICAL CLEARANCE AT
PASSENGER LOADING ZONE

PASSENGER LOADING ZONES

If passenger loading zones are provided adjacent to drop-off areas, they should be a minimum of four feet (4') wide by twenty feet (20') long to accom-

modate mobility aids and the full extension of van lifts. Curb ramps should be provided in compliance with curb ramp specifications. In addition, a minimum height clearance of nine feet, six inches (9'-6") should be provided to accommodate vans which may need extended headroom for disabled drivers and passengers in wheelchairs. Ground surfaces should comply with the requirements for grade and surfaces stated earlier in the section on parking.

BUS STOPS

Transportation districts are providing an increasing number of buses with lifts for disabled passengers. The lifts are designed to accommodate not only persons in wheelchairs, but everyone who has difficulty in mounting bus steps.

Bus stops along a bus route should be designed or modified to ensure that disabled passengers will be able to unload successfully, as well as be able to enter an accessible pedestrian route. In order to ensure that a bus stop is accessible, the following criteria should be considered.

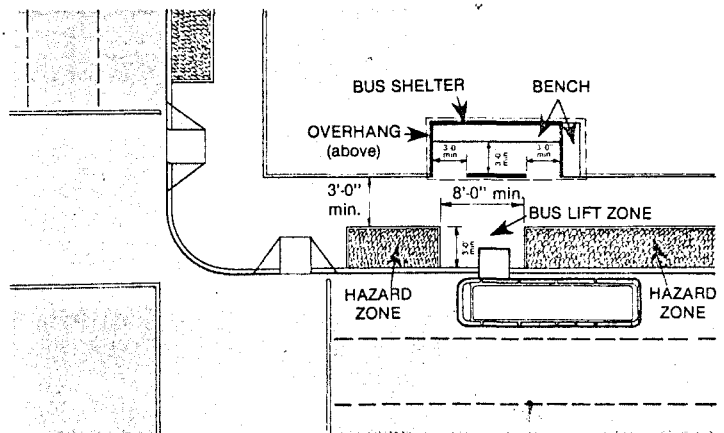


FIGURE 17
ACCESSIBLE BUS STOP

Lift Requirements:

- The street adjacent to the bus stop should not have a high crown which will cause the bus and lift to tilt.
- The curb upon which the lift comes to rest should be low enough to allow the full deployment of the lift. Eight inches (8") should be the maximum height for these curbs.
- Obstructions such as shelters, newsstands, benches, planters, trees, mailboxes, weeds, guy wires, or sprinkler heads should be eliminated, because they can prevent the full deployment of the lift.

Bus Zone Requirements:

- A flat, paved surface sufficient for the full deployment of the lift should be provided. This pad should be approximately eight feet (8') long (in a direction parallel with the street) and at least five feet (5') wide.
- The drop-off surface should be connected to the pedestrian pathway via an accessible route, and the pedestrian pathway should lead to intersections with curb ramps.
- Bus shelters, newsstands, and other structures within the bus zone should not reduce the width of the pedestrian pathway to less than thirty-six inches (36").

Bus Shelters:

- Bus shelter entrances should be a minimum of thirty-two inches (32") in width. A thirty-six inch (36") width is preferable.
- Bus shelters with one entrance should have an interior open space sixty inches by sixty inches (60"x60"). To provide sufficient maneuvering space for a person in a wheelchair, it should be clear of obstructions.
- Bus shelters with two entries should have a continuous unobstructed path thirty-six inches (36") wide connecting the doorways.
- Bus shelters should allow an unobstructed view of arriving vehicular traffic for standing and seated persons.
- Bus shelter seats should be provided at a height of seventeen inches (17") above the ground.

CONCLUSION

The environment will never be truly barrier free until it is possible for anyone to travel in-

dependently from one place to another. This bulletin has attempted to outline design criteria for some of the more important elements in an accessible pedestrian/vehicular environment.

RESOURCES

Specifications for Making Buildings and Facilities Accessible to and Usable by Physically Handicapped People, ANSI A117.1-1980, Available from the American National Standards Institute, Inc., 1430 Broadway, New York, NY 10018

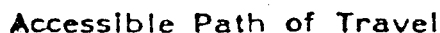
Minimum Guidelines and Requirements for Accessible Design, Available from the United States Architectural and Transportation Barriers Compliance Board, Washington, D.C. 20201

Hollyn Fuller is an accessibility specialist working with Access California in Sacramento. Ms. Fuller has been instrumental in the development of that state's accessibility regulations especially as they relate to historic facilities.

This Access Information Bulletin has been made possible through funding under contract #300-80-0681, Office for Civil Rights, U.S. Department of Education. The opinions expressed herein are those of the author and should not be construed as representing the opinions or policy of any agency of the United States Government or the National Center for a Barrier Free Environment.

Except for those items indicated with an asterisk (*), the information in this bulletin reflects the intent or details of the ANSI Standard A117.1 (1980) *Specifications for Making Buildings and Facilities Accessible to and Usable by Physically Handicapped People*. Inquiries and/or information regarding this bulletin should be directed to the Design Resources Coordinator, National Center for a Barrier Free Environment, Suite 1006, 1140 Connecticut Avenue, N.W., Washington, D.C. 20036.

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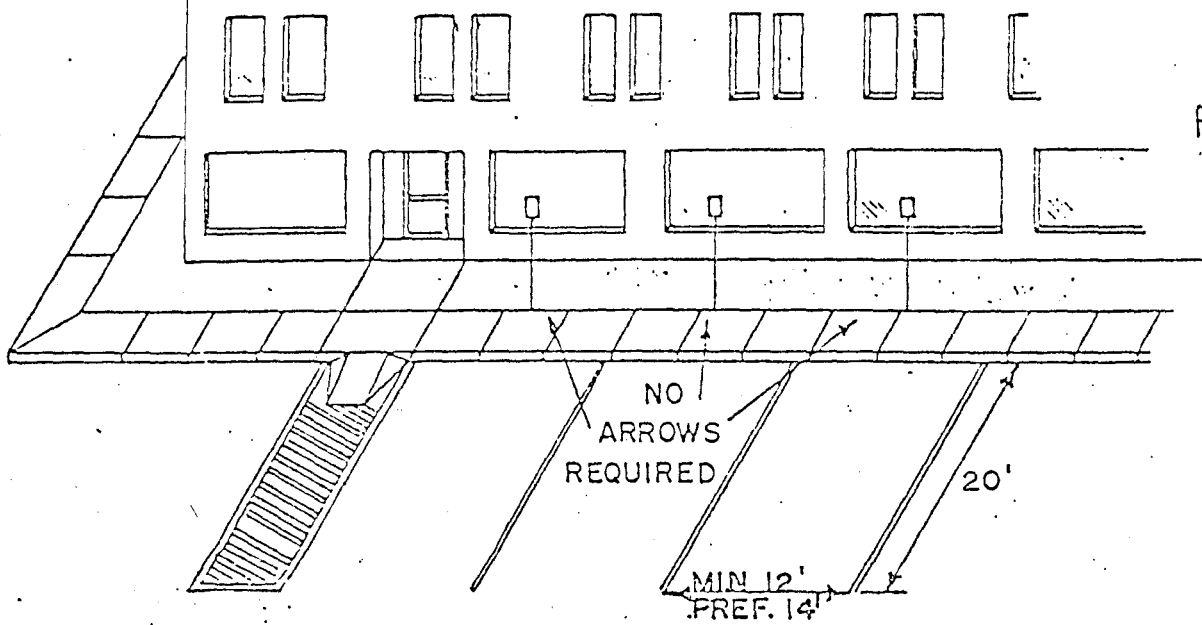


- From each handicapped parking space to related facilities.
- No ramps to accessible path of travel to encroach in parking area.
- Ramps in front of handicapped parking space loading areas may project into load area provided they still allow handicapped persons to leave and enter vehicle.
- Provide curbs or wheel stop bumpers to keep autos from projecting over required 4' wide walks.
- Minimum vertical clearance of 8'-2" to parking structure entrances or overhead structure, or "equivalent facilitation," when enforcing agency determines that compliance to regulation or "equivalent facilitation" creates unreasonable hardship in existing parking structures, vertical clearance shall not apply.

- Individual spaces 14 ft. wide lined to provide a 9 ft. parking space and 5 ft. loading area.
- Two spaces 23 ft. wide lined to provide two 9 ft. parking spaces and one shared 5 ft. loading area.
- Minimum depth of parking space 18'-0".

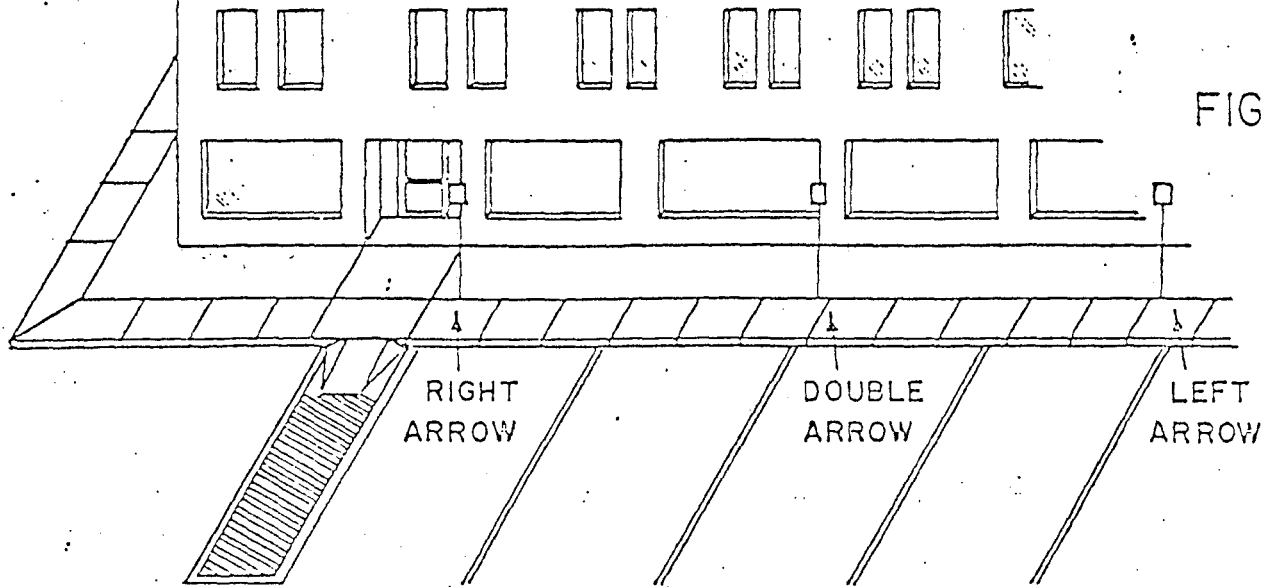
SIGNING ONE, TWO, OR THREE SPACES.

FIGURE II



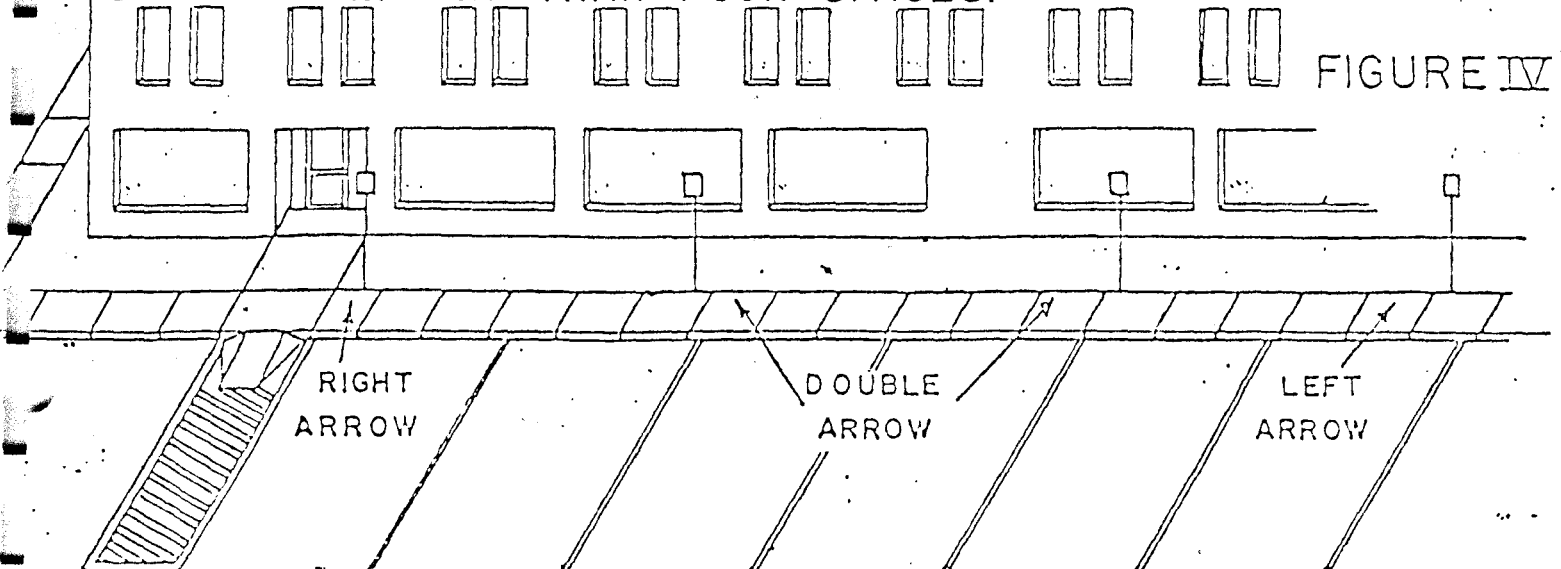
SIGNING FOUR SPACES.

FIGURE III



SIGNING GREATER THAN FOUR SPACES.

FIGURE IV



I would like to go on record in support of Senate Bill 93, which will require cities, towns, and counties to follow specifications as to the dimensions of handicapped parking spaces.

The awareness for the need of handicapped parking is finally being realized. I feel, however, cities and towns need to have some direction as to the specifications and dimensions of the parking spaces. The language in SB 93 is right in line with the Federal Architectural and Transportation Barriers Compliance Board. A handicapped parking space does no good if a person does not have enough room to get their wheelchair out of a car or to lower lift in a van.

I do feel since you are talking about dimensions of parking spaces it would be appropriate to include language pertaining to the signs indicating a handicapped parking space. It would be a great help if you were to put a minimum size of the sign, perhaps 18 inches by 24 inches and to state the sign must be on a pole or wall and visible whether or not a vehicle is parked there. This would get away from painting or stencilling the International Symbol of Access of the ground. This is just no good in Montana when the symbol is under two inches of snow half of the year. I also feel this would aid law enforcement officers to detect if someone is parked illegally in a designated space. If the car is parked over the sign, it's hard to tell there is one there at all.

Thank you for this time to testify, and if I may provide further information in support of SB 93, please let me know.

Robert D. Liston

Robert D. Liston

513½ State St.

Helena, MT 59601

443-1089 or 444-3886

*This is the opinion of the Governor's Committee on
Employment of the Handicapped*

01-17-85

LOCAL GOVERNMENT

VISITORS' REGISTER

[illegible]

(Please leave prepared statement with Secretary)

LOCAL GOVERNMENT COMMITTEE
JANUARY 17, 1985

NAME Robert D. Liston BILL NO. SB 93

ADDRESS 513 1/2 State Helena DATE 1-17-85

WHOM DO YOU REPRESENT Gov. Comm. on Employment of the Handicapped

SUPPORT ✓ OPPOSE _____ AMEND _____

PLEASE LEAVE PREPARED STATEMENT WITH SECRETARY.

Comments:

Prepared statement with secretary.

LOCAL GOVERNMENT COMMITTEE
JANUARY 17, 1985

NAME Robert D. Loston BILL NO. SB 92 ~~98~~

ADDRESS 513 1/2 State Helena DATE 1-17-85

WHOM DO YOU REPRESENT Governor's Committee on Employment of the Handicapped

SUPPORT ✓ OPPOSE _____ AMEND _____

PLEASE LEAVE PREPARED STATEMENT WITH SECRETARY.

Comments:

Prepared statement with secretary